

INFORMATION REPORT

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| COUNTRY | East Germany / Poland | REPORT | | 25X1 |
| SUBJECT | Proposed Changes in the East German Shipbuilding Industry | DATE DISTR. | 17 February 1955 | |
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THE SOURCE EVALUATIONS IN THIS REPORT ARE DEFINITIVE.
THE APPRAISAL OF CONTENT IS TENTATIVE.
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1. The report on the Polish shipbuilding industry which was given by the members of the East German delegation which visited Poland in early October 1954¹, although extravagant in its praise of the efficiency of the Polish industry, was given by the members of the delegation in all seriousness and they seemed to believe what they were reporting. The report was written and submitted in person to Heinrich Rau, East German Minister for Machine Construction, by two or three members of the delegation, who at the same time submitted a proposal consisting of sixteen points to be applied to the East German shipbuilding industry in order to improve it in accordance with the Polish industry, which was taken as a model.
2. As a result, Minister Rau decided that another group, consisting of 12 employees of the Main Administration for Shipbuilding, should make a trip to Poland lasting several weeks in order to study Polish methods in detail and to determine whether the sixteen points could actually be implemented in the East German shipbuilding industry. However, it would be impossible for the group to go to Poland before the end of 1954 because they could not be spared from their duties connected with the fulfillment of the production plan.
3. The sixteen points proposed by the delegation members to Minister Rau were as follows:
 - a. Shipbuilding production in East Germany should be standardized, and its various technical branches should be assigned to the individual shipyards according to the facilities at each yard. That is, construction of new ships should not be done at a yard which also does repair work.

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- b. A feasible production plan should be submitted via government agencies, as is done in Poland. The planning for coordination within the framework of shipbuilding planning should be set up to eliminate any imbalance.
- c. Structural and functional standards should be established for all shipyards, according to which the responsible officers of the yards would supply examples (as a basis) for the preparation of production. Coordination with members of the delegation that visited Poland should be carried on.
- d. The method of handling the drawing up of plans for shipbuilding projects used heretofore should be definitely abandoned. A technical and orderly reorganization is necessary. The exchange of patterns with the Central Plans Office of the Polish shipbuilding industry should be instituted for the purpose of increasing the 9 ratifications of the workers in the East German Central Plans Office (ZKB). The Central Plans Office should achieve self-sufficiency - it should not play the part of a supporting institute.
- e. An effort must be made to create respect for technical work and strict adherence to technical requisites. Technical-economic investigation by the Institute for Shipbuilding (Institut fuer Schiffbau) is needed in order to provide guidance for the shipyards and main administrations.
- f. Trade agencies should be required to conclude contracts solely when these have been confirmed by the Zentrales Konstruktions-Büro (Central Designing Office) (ZKB) and the shipyards. Serial construction should be started, i.e. the first and following four ships should be built without any changes. Suggestions for modifications should be given consideration only after the construction of every five, or every ten ships.
- g. An exchange of personnel with Poland should take place, with Germans working in Polish shipyards and vice versa for certain periods of time, for the purpose of mutual encouragement to do good work.
- h. Exchange of technical plans for the construction of the 10,000-ton freighters. Also there should be an exchange of plans for preliminary work and wage groups (Lohngruppen)².
- i. A qualified group (Kollektiv) should investigate, with the cooperation of members of the delegation:
 - a) The apportioning of the tasks of the chief mechanic and of the official in charge of the employment of technical equipment (Anlagentechnik)
 - b) The division of the duties of the chief engineer, i.e., technical chief, among individual departments.
- j. In all the shipyards, including repair yards, company funds should be made available for the improvement of social welfare.
- k. (1) The establishment of a professorship for the technology of shipbuilding and the construction of marine engines should be carried out.
- (2) A systematic research plan in the field of shipbuilding and the construction of marine engines should be carried out.
- (3) Measures should be introduced which would lead to the completion of suitable shipyard installations (Einrichtungen) and appliances (Vorrichtungen). One job should be completed before another is begun, and methods should be introduced in the shipbuilding industry to improve work in the final assemblage enterprises.
- (4) An exchange with Professor Derwer (?), dean of the shipbuilding school at the Technical College in Poland should be carried out. That is, a professor should be sent to Poland in exchange for Derwer (?).

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- l. Definite invitations to visit East Germany should be issued to representatives of the Polish shipbuilding industry.
- m. Detailed investigations as to whether shipyard activities could be separated into the two categories of repair work and the construction of new vessels should be carried out. Also, the investments yet to be carried out should be in accordance with this principle, if it is adopted. Increased mechanization should be introduced.
- n. Measures should be adopted to simplify working methods. It is suggested that the shipyards be assigned to the following Ministries:

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| Construction of new vessels | Responsible or subordinate to the Ministry for Machine Construction |
| Fleet repairs | Ministry for Food Industry or, in part, the Ministry of the Interior |
| Construction of river vessels | Ministry for Transport (Verkehr) |

- o. The planning should be divided as follows:

- a) Project planning
- b) Coordination planning (Kooperationsplanung).
- c) Production planning.

Changes in the plan for the plan year and the Five-Year Plan should be avoided. The Main Administration should use its influence to see to it that contracts cover long periods of time.

- p. More Polish delegations should be invited to technical conferences on shipbuilding in East Germany. The tasks of the shipbuilding institutes and the shipbuilding schools should be made more concrete. Agreements on the production program should be made among the nations of the "peace camp".

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CENTRAL INTELLIGENCE AGENCY
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|----------------|---|-----------------|--|------|
| COUNTRY | East Germany | REPORT | <div style="border: 1px solid black; width: 150px; height: 20px;"></div> | 25X1 |
| SUBJECT | Extraction of Germanium from Zinc Oxide at Feinzink Freiberg and the NE-Forschungsinstitut Freiberg | DATE DISTR. | 24 February 1955 | |
| | | NO. OF PAGES | 1 | |
| DATE OF INFO. | <div style="border: 1px solid black; width: 200px; height: 40px;"></div> | REQUIREMENT NO. | RD | 25X1 |
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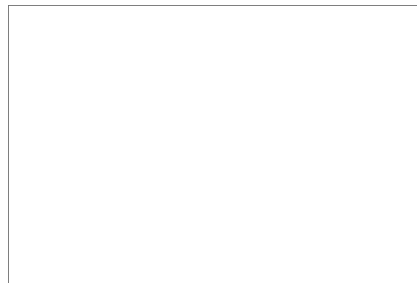
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1. In Mildenhuetten there is a laboratory in which it is planned to produce germanium from Mansfeld zinc oxides. Twenty to twenty-five kg of this material, which is needed for the high frequency industry, is required annually from the Soviet Union. At present, however, the electrolysis is still insufficient and zinc sulphate still remains to be manufactured in sufficient quantity. So far, the Mansfeld zinc oxides have been absorbed by the dye industry.
2. The Nonferrous Research Institute in Freiberg, an independent organization, is under the direction of Dipl. Ing. Peukert (fnu) and Dr. Schreiter (fnu). This institute is producing a monthly quantity of 500 grams of germanium oxide from which it is possible to extract about 2/3 germanium. This extraction, however, is done at some other location. It is planned to install a copper contact installation at the Nonferrous Research Institute for the extraction of germanium.

1. Comment. This laboratory is part of the VEB Feinzink, 51, Frauenstrasse 51, Freiberg, , the remainder of a project which was to have been activated in 1953 for the installation of a zinc smeltery. The project was to be installed at the cost of 150 million DME , but is now in abeyance. 25X1



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